OWNER/APPLICANT

PORT OF HOOD RIVER 1000 E. PORT MARINA DRIVE HOOD RIVER, OR 97031 PHONE (541) 386-1645

ENGINEER/SURVEYOR

900 WEST STEUBEN STREET, P.O.B. 308, BINGEN, WA. 98605 PHONE (509) 493-3886 FAX (509) 493-3885

HATCH AND SYMBOL LEGEND

PORT OF HOOD RIVER

PORTWAY STORMWATER

PIPELINE PROJECT

HOOD RIVER, OREGON

JULY 2022

EX. HYDRANT SANITARY CLEANOUT

EX. SANITARY MANHOLE

EX. GUY WIRE

EX. UTILITY POLE

EX. STREETLIGHT

EX. TELEPHONE / COMM. PEDESTAL

EX. VAULT EX. WATER VALVE

EX. WATER METER

EX. TRANSFORMER

EX. STORM MANHOLE

SURVEY NOTES

1) DATUM AS PER NAD 83, NAVD 88 - PONDER 2 NGS BENCHMARK

2) TOPOGRAPHIC DATA AS PER FIELD TIES AND AERIAL PHOTOGRAPHY

3) CONTOURS @ 1' INTERVALS

4) LOCATION OF UTILITIES AS PER LOCATES, FIELD TIES, AS-BUILT DRAWINGS, AND VISUAL INSPECTION - CONTRACTOR TO VERIFY LOCATION

LINETYPE LEGEND

— EX. POWER UNDER GROUND

-- PROPERTY BOUNDARY

T U G ---- EX. TELEPHONE / CABLE UNDERGROUND

EX. SIDEWALK

EX. ASPHALT/CONC. AREA

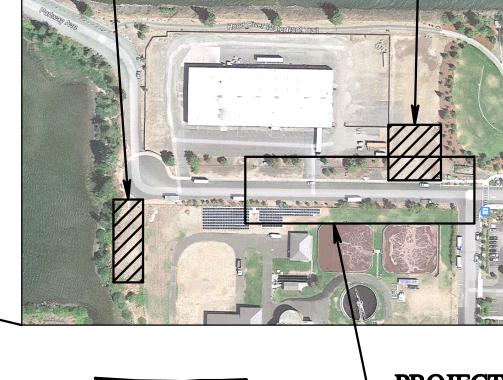
-----S EX. STORM LINE

── G ── EX. 2" GASLINE

___x___x___x_ EX. FENCE



CONSTRUCTION STAGING AREA



WHITE SALMON HOOD RIVER

PROJECT

WWTP = WASTEWATER TREATMENT PLANT LOCATION UND

= YELLOW PLASTIC CAP **LOCATION NOTES**

PCT

STA

THKN

-PORTIONS OF SECTION 25, TOWNSHIP 3 NORTH, RANGE 10 EAST, W.M. -PORTIONS OF THE COE DONATION LAND CLAIM -MULTIPLE AGENCY OWNERSHIP INCLUDING: CITY OF HOOD RIVER PORT OF HOOD RIVER

THIS LINE IS 1" LONG. IF THIS LINE IS NOT 1"LONG, ADJUST SCALE ACCORDINGLY

ABBREVIATIONS

= EXISTING OR EAST

= ASPHALT CONCRETE

= ASPHALT CONCRETE PAVEMENT

= AMERICAN CONCRETE INSTITUTE

HIGHWAY AND TRANSPORTATION OFFICIALS

= AMERICAN WATER WORKS ASSOCIATION

= AMERICAN SOCIETY FOR TESTING AND MATERIALS

AASHTO = AMERICAN ASSOCIATION OF STATE

= BEST MANAGEMENT PRACTICE

= CRUSHED SURFACING TOP COURSE

= EROSION & SEDIMENT CONTROL

= INTERNATIONAL BUILDING CODE

= NORTH AMERICAN VERTICAL DATUM

ODOT = OREGON DEPARTMENT OF TRANSPORTATION

= EROSION & SEDIMENT CONTROL PLAN

= AMERICAN WIRE GAUGE

= ELEVATION

= GATE VALVE

= MANHOLE

= NOT TO SCALE

= NORTHWEST

= PERCENT

= SOUTH

= STATION

= SOUTH WEST

= THICKNESS

= WATER VALVE

= TYPICAL

= WEST

= TOP OF GRATE

= TOP OF PAVEMENT

= NORTH

= EDGE OF CONCRETE

= HOOD RIVER CITY

= INVERT ELEVATION

= H.R.FIRE DEPARTMENT

= NORTH AMERICAN DATUM

= POINT OF INTERSECTION

= REMOTE TELEMETRY UNIT

= SIDEWALK, CURB, AND GUTTER

= UNLESS OTHERWISE APPROVED

TOTAL LINEAR FEET

OF IMPROVEMENTS

85 LF 10" ASTM D3034 PVC

47 LF 12" ASTM D3034 PVC

326 LF 18" ASTM F679 PVC

= UNLESS NOTED OTHERWISE

= RED PLASTIC CAP = RESILIENT WEDGE

= RIGHT OF WAY

= STORM DRAIN = SOUTH EAST

= SANITARY SEWER = STORM SEWER

= PUBLIC UTILITY EASEMENT

= NATIONAL GEODETIC SURVEY

= EAST

SHEET LEGEND

COVER SHEET NOTES

EXISTING CONDITIONS PROPOSED STORMLINE

HOOD RIVER STANDARD DETAILS

HOOD RIVER STANDARD DETAILS

C 2.03 DETAILS

WORK HOURS AND ACCESS

-WORK HOURS SHALL BE 7:00 AM TO 7:00 PM MONDAY - FRIDAY EXCLUDING HOLIDAYS.

-CONTRACTOR SHALL MAINTAIN ACCESS TO ALL RESIDENT AND BUSINESSES AT ALL TIMES DURING CONSTRUCTION

DESIGN EXCEPTION

THE PORT OF HOOD RIVER HAS REQUESTED A DESIGN EXCEPTION TO THE CITY OF

CALL BEFORE YOU DIG

ONE CALL UTILITY LOCATE (OREGON) - (800) 332-2344

ATTENTION EXCAVATORS: OREGON LAW REQUIRES THE CONTRACTOR TO FOLLOW RULES ADOPTED BY THE OREGON UTILITY NOTIFICATION CENTER.

THOSE RULES ARE SET FORTH IN OAR 952-001-0010 THROUGH OAR 952-001-0090 THE CONTRACTOR MAY OBTAIN COPIES OF THESE RULES FROM THE CENTER BY

CALLING (503) 232-1987 OR BY ACCESSING THE INTERNET AT: WWW.STATE.OR.US/GOVERNMENT.HTM.

THE CONTRACTOR MUST NOTIFY THE CENTER AT LEAST 2 BUSINESS DAYS, BUT NOT MORE THAN 10 BUSINESS DAYS, BEFORE COMMENCING AN EXCAVATION CALL 811.

UTILITY/AGENCY CONTACTS:

CITY OF HOOD RIVER PUBLIC WORKS 1200 18TH ST. HOOD RIVER, OR 97031 WADE SEABORN - (541) 387-5201

PACIFIC POWER AND LIGHT CO. ALAN WILM - (541) 308-2008 P.O. BOX 300, HOOD RIVER, OR 97031

CENTURYLINK (LUMEN) 4501 MINNEHAHA ST. BLDG. 2 VANCOUVER, WA 98661 MARCO GALAS - (564) 888-2027

NORTHWEST NATURAL GAS CO. 1125 BARGEWAY RD. THE DALLES, OR 97058 TANYA BRUMLEY - (541) 296-2229 EXT. 8610

CHARTER COMMUNICATIONS ERNIE GARCIA - (541) 288-6492 409 UNION STREET, THE DALLES, OR 97058

HOOD RIVER FIRE DEPARTMENT LEONARD DAMIAN - (541) 386-3939 1785 MEYER PKWY, HOOD RIVER, OR 97031

HOOD RIVER ELECTRIC CO-OP CLINTON CURTIS - (541) 354-1233 3521 DAVIS DRIVE, ODELL, OR 97044-0125

GORGE NETWORK 616 INDUSTRIAL AVE. HOOD RIVER, OR 97013 DAVE KELEHER - (541) 436-0214 EXT. 214

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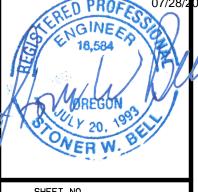


ORTWAY STO PIPELINE P

^{ЈОВ NO.} 21В295 DATE 07/25/22

REVISIONS

RECORD DRAWINGS



						SIGNOFF &
TEST	STANDARD	FREQUENCY	TESTING AGENCY	TIMING	TEST REQUIREMENTS	DATE
SUBGRADE CUT SECTION COMPACTION	AGENCY/ENGINEER	ALL	AGENCY*4*5	PRIOR TO BASE ROCK	LOADED 10 CY TRUCK-PROOFROLL	
SUBGRADE FILL COMPACTION	AASHTO T-180	EVERY 2500 CY, MIN. 2 & 1 PER 3' OF DEPTH	CERTIFIED LAB	FIELD REPORTS REQUIRED PRIOR TO BASE ROCK	95% MIN. COMPACTION	
SODOTADE TIEL COM ACTION	AGENCY/ENGINEER	ALL	AGENCY*4*5	PRIOR TO BASE ROCK	LOADED 10 CY TRUCK-PROOFROLL	
UTILITY BACKFILL COMPACTION *2 (IN RIGHT OF WAY)	AASHTO T-180	EVERY 200', MIN 1/LINE; 2 @ 100' FOR FAILED TESTS *1	CERTIFIED LAB	REPORTS PRIOR TO BASE ROCK	95% MIN. COMPACTION	
UTILITY BACKFILL COMPACTION *2 (OUT OF RIGHT OF WAY)	AASHTO T-99 or AASHTO T-180	EVERY 200', MIN 1/LINE; 2 @ 100' FOR FAILED TESTS*1	CERTIFIED LAB	PRIOR TO FINAL ACCEPTANCE	MINIMUM COMPACTION REQUIRED: 95% W/ T-99 OR 90% WITH T-180	
UTILITY BACKFILL COMPACTION *2 (OVER 3 FT. IN DEPTH)	SAME AS ABOVE REQUIREMENTS	EVERY 5' OF DEPTH & SAME AS ABOVE REQUIREMENTS	SAME AS ABOVE REQUIREMENTS	SAME AS ABOVE REQUIREMENTS	SAME AS ABOVE REQUIREMENTS	
LOT AREA COMPACTION (NON-TRAFFIC AREAS)	AASHTO T-99 or AASHTO T-180	MIN. 1 PER EVERY 3' DEPTH, EACH LOT	CERTIFIED LAB	PRIOR TO FINAL ACCEPTANCE	MINIMUM COMPACTION REQUIRED: 95% W/ T-99 OR 90% WITH T-180	
WATER MAINS DISINFECTION	AWWA C651	ALL	CONTRACTOR*5	PRIOR TO BACTERIA SAMPLE	AWWA C651 5.2.3.3 METHOD	
BACTERIA WATER SAMPLE	AWWA C651	2 CONSECUTIVE TEST 24 HR APART PER REQ. LOCATION MIN. 2 LOCATIONS PER MAIN. 1 ADDITIONAL PER BRANCH AND 1 ADDITIONAL PER 1,000 FT.	ADDY LAB OR APPROVED LAB	PRIOR TO CONNECTION TO MAIN & BASE ROCK	E. COLI & COLIFORM ABSENT	
WATER LINE LEAK TEST	AWWA C605 AWWA C600 ASTM F2164	MAIN, SERVICE, F.H., AIR/VAC	CONTRACTOR*5	PRIOR TO BASE ROCK	C605 7.3 SIMULTANEOUS PRESSURE AND LEAK- AGE TEST OR C600 5.2. 2HR @ MAX OF 150% OF STATIC OR 150 PSI, WHICHEVER IS GREATER.	
SEWER PRESSURE MAIN LEAK TEST	AWWA C605 AWWA C600 ASTM F2164	MAIN, SERVICE, AIR/VAC	CONTRACTOR*5	PRIOR TO BASE ROCK	C605 7.3 SIMULTANEOUS PRESSURE AND LEAK- AGE TEST OR C600 5.2. 2HR @ MAX OF 150% OF STATIC OR 150 PSI, WHICHEVER IS GREATER.	
LOW PRESSURE SEWER PRESSURE TEST	AGENCY/ENGINEER	MAIN, AIR/VAC	CONTRACTOR*5	PRIOR TO BASE ROCK	15 MIN. @ 150 PSI (MIN) OR WORK PRESSURE (HIGHER OF 2). NOT TO EXCEED 200 PSI.	
STEP SEWER SERVICE	AGENCY/ENGINEER	ALL SERVICES	CONTRACTOR	PRIOR TO BASE ROCK	100 PSI FOR 30 SECONDS	
STEP / STEF SEWER TONE TEST	AGENCY/ENGINEER	MAIN, SERVICES	AGENCY *4 *5	PRIOR TO BASE ROCK	CONTINUOUS TONE	
GRAVITY SEWER - AIR TEST	ASTM F 1417	MAIN, SERVICES	CONTRACTOR*5	PRIOR TO BASE ROCK	PER ASTM F 1417-92	
STEF MH COATING	AGENCY/ENGINEER	ALL		AFTER VACUUM TEST	H ₂ S RESISTANT - CERTIFIED APPLICATOR - SUBMITTAL REQ'D	
VACUUM TEST - MH	ASTM C1244	ALL	CONTRACTOR*5	AFTER PAVEMENT INSTALLED	PER ASTM C1244-05A	
HYDROSTATIC TEST - MH	ASTM C969	ALL	CONTRACTOR*5	AFTER PAVEMENT INSTALLED	0.019 GPH/FT DIAM/FT OF DEPTH OF MH	
STEF/CONVENTIONAL SEWER - MANDREL TEST	ODOT 00445	AT AGENCY REQUEST	CONTRACTOR*5	PRIOR TO BASE ROCK	EFF LENGTH > D, MANDREL DIA > 0.95D	
STEF/CONVENTIONAL SEWER - TV TEST	ODOT 00445	MAIN REQUIRED	CONTRACTOR	PRIOR TO BASE ROCK	INSPECTION REPORT & VIDEO	
STORM SEWER MANDREL TEST	ODOT 00445	AT AGENCY REQUEST	CONTRACTOR*5	PRIOR TO BASE ROCK	EFF LENGTH > D, MANDREL DIA > 0.95D	
STORM SEWER TV TEST	ODOT 00445	AT AGENCY REQUEST	CONTRACTOR	PRIOR TO BASE ROCK	INSPECTION REPORT & VIDEO	
BASE ROCK GRADATION	ODOT 00610	ONE PER SOURCE	CERTIFIED LAB	PRIOR TO PLACEMENT	COPY TO INSPECTOR	
BASE ROCK COMPACTION	AASHTO T-180	EVERY 250'	CERTIFIED LAB	PRIOR TO TOP ROCK, CURBS, DRIVEWAYS, APPROACHES, AND SIDEWALKS	95% MIN. COMPACTION	
TOP ROCK GRADATION	ODOT 00610	ONE PER SOURCE	CERTIFIED LAB	PRIOR TO PLACEMENT	COPY TO INSPECTOR	
TOP ROCK COMPACTION	AGENCY/ENGINEER	ALL	AGENCY*4*5	PRIOR TO PLACEMENT	LOADED 10 CY TRUCK-PROOFROLL	
AC GRADE & OIL CONTENT	ODOT 00744	ONE PER SOURCE	CERTIFIED LAB	PRIOR TO PLACEMENT	COPY TO INSPECTOR	
AC COMPACTION	AASHTO T-209	ONE EVERY 1000 TONS * (3)	CERTIFIED LAB	DURING PLACEMENT	1ST LIFT 91% MIN. COMPACTION. SUBSEQUENT LIFTS 92% MIN. COMPACTION	
SUBGRADE GRADE CHECK ELEV	AGENCY/ENGINEER	PER STATION (0+25)	AGENCY*4*5	PRIOR TO BASE ROCK	+0.04' TOLERANCE	
BASE ROCK GRADE CHECK ELEV	AGENCY/ENGINEER	PER STATION (0+25)	AGENCY*4*5	PRIOR TO TOP ROCK	MINIMUM SECTION REQUIRED	
TOP ROCK GRADE CHECK ELEV	AGENCY/ENGINEER	PER STATION (0+25)	AGENCY*4*5	PRIOR TO PAVEMENT	+ 0.02' TOLERANCE	
CONCRETE CURB	ACI	PER 100', 1 TEST MIN.	CERTIFIED LAB	PRIOR TO TRAFFIC USE	28 DAY STRENGTH	
CONCRETE WALLS, STRUCTURAL ITEMS	ACI	PER ITEM AND EVERY 16 CYS	CERTIFIED LAB	PRIOR TO LOADING	28 DAY STRENGTH	
CONCRETE DRIVEWAYS, APRONS, APPROACHES	ACI	PER EACH AND EVERY 60 SY	CERTIFIED LAB	PRIOR TO TRAFFIC USE	28 DAY STRENGTH	

NOTE: CONTRACTOR SHALL BE RESPONSIBLE FOR ALL COSTS ASSOCIATED WITH TESTING. ALL TESTING COST SHALL BE CONSIDERED INCIDENTAL AND INCLUDED IN THE APPROPRIATE UNIT BID ITEM.

* 1) NOT LESS THAN ONE TEST BETWEEN UTILITY STRUCTURES (MHs, CLEANOUTS, VAULTS, ETC.)

2) ALL BACKFILL AROUND UTILITY STRUCTURES SHALL BE COMPACTED AND TESTED, ONE TEST PER STRUCTURE.

3) ONE TEST SHALL BE CONDUCTED EVERY BLOCK OR 500 LF OF WORK AS DETERMINED BY THE ENGINEER. 4) AGENCY SHALL BE OWNER APPROVED, INDEPENDENT, 3RD PARTY LAB OR TESTING AGENCY

5) ENGINEER OR PORT REPRESENTATIVE SHALL BE PRESENT TO OBSERVE FIELD TESTS OR COLLECTION OF SAMPLES.

OREGON STANDARD TESTING

OTHERWISE BE CONSIDERED INCIDENTAL TO THIS PROJECT.



THIS PROJECT IS ONE ELEMENT OF A LARGER PROJECT TO IMPROVE STORMWATER INFRASTRUCTURE WITHIN THE WATERFRONT DISTRICT. OTHER CONTRACTS AND CONSTRUCTION MAY BE TAKING PLACE DURING THE CONSTRUCTION PERIOD OF THIS CONTRACT. IF OTHER CONSTRUCTION IS OCCURRING, THE CONTRACTOR SHALL TAKE ALL MEASURES NECESSARY TO COORDINATE THIS CONSTRUCTION WITH THE OTHER CONSTRUCTION ACTIVITIES. THIS COULD EFFECT SITE ACCESS AND TIMING OF CONNECTIONS. COSTS TO COORDINATE WITH OTHER CONSTRUCTION ACTIVITIES AND OTHER AGENCIES

(CITY OF HOOD RIVER, OR THEIR AGENTS) SHALL BE INCLUDED IN THE MOBILIZATION COST AND

C0.01

COORDINATION WITH OTHER CONTRACTORS AND AGENCIES

THIS LINE IS 1" LONG.

1.0 ROCK EXCAVATION

A. ROCK EXCAVATION IS DEFINED AS THE REMOVAL OF ALL MATERIAL WHICH BY ACTUAL DEMONSTRATION, CANNOT, IN THE ENGINEER'S JUDGMENT, BE REASONABLY EXCAVATED WITH EQUIPMENT COMPARABLE TO TYPES LISTED IN TABLE 1 AND EQUIPPED WITH RIPPERS OR SIMILAR APPROVED EQUIPMENT AND WHICH IS, IN FACT, SYSTEMATICALLY DRILLED OR BROKEN BY POWER-OPERATED TOOLS DESIGNED FOR ROCK EXCAVATION. ENGINEER MAY WAIVE THE DEMONSTRATION IF MATERIAL ENCOUNTERED IS WELL-DEFINED ROCK. THE TERM ROCK EXCAVATION SHALL BE UNDERSTOOD TO INDICATE A METHOD OF REMOVAL AND NOT A GEOLOGICAL FORMATION.

TARLE 1

TADLE 1								
	MANUFACTURER	MODEL	MINIMUM NET HORSE POWER	TYPE OF EXCAVATION				
	CATERPILLAR	225	125	TRENCH				
	JOHN DEERE	690	125	TRENCH				
	CASE	125B/980B	125	TRENCH				

B. ENGINEER WILL MAKE DETERMINATION OF "ROCK EXCAVATION"

C. SOLID ROCK IS DEFINED AS FOLLOWS:

1. SOLID ROCK SHALL COMPRISE AND INCLUDE ALL ROCK MATERIAL WHICH IS IN LEDGES, BEDDED DEPOSITS AND UNSTRATIFIED MASSES AND WHICH CANNOT BE REMOVED WITHOUT DRILLING AND CONGLOMERATE DEPOSITS WHICH ARE SO FIRMLY CEMENTED AS TO PRESENT ALL THE CHARACTERISTICS OF SOLID ROCK AND WHICH CANNOT BE REMOVED WITHOUT DRILLING AND BLASTING.

2. WHEN SOLID ROCK LAYERS HAVE AN OVERBURDEN OF MATERIAL OF COMMON CLASSIFICATION WHICH CANNOT PRACTICALLY BE STRIPPED AND HANDLED SEPARATELY FROM THE SOLID ROCK AND/OR ARE INTERSPERSED WITH A MATERIAL OF COMMON CLASSIFICATION, THE ENTIRE MASS WILL BE CLASSIFIED AS SOLID ROCK IF THE SOLID ROCK CONSTITUTES NOT LESS THAN 85 PERCENT OF THE ENTIRE MASS.

3. BROKEN AND FRACTURED ROCK WILL NOT BE CLASSIFIED AS ROCK NOR WILL SO-CALLED "HARD PAN" OR CEMENTED GRAVEL, EVEN THOUGH IT MAY BE ADVANTAGEOUS TO USE ROCK EXCAVATION METHODS IN ITS

4. BOULDERS MEASURING LARGER THAN 1 CUBIC YARD IN VOLUME WILL BE CLASSIFIED AS ROCK IF DRILLING OR OTHER APPROVED METHODS ARE ACTUALLY USED FOR THEIR REMOVAL FROM THE TRENCH. IF MATERIAL WHICH WOULD BE CLASSIFIED AS ROCK BY THE ABOVE DEFINITION IS MECHANICALLY REMOVED WITHOUT BREAKING, OR SPLITTING, IT WILL NOT BE CONSIDERED AS ROCK EXCAVATION. IF EQUIPMENT LARGER THAN THAT PREVIOUSLY IDENTIFIED IS SPECIFICALLY BROUGHT IN FOR THE SOLE PURPOSE OF ROCK REMOVAL, AS DEFINED ABOVE, THEN SUCH REMOVAL WILL BE CONSIDERED ROCK EXCAVATION AND WILL BE PAID FOR AS SUCH AT THE UNIT PRICE BID.

D. BEFORE ROCK REMOVAL BY SYSTEMATIC DRILLING OR OTHER METHODS WILL BE PERMITTED, NOTIFY ENGINEER WHO, WITH CONTRACTOR OR ITS REPRESENTATIVE, WILL DETERMINE THE AMOUNT OF MATERIAL TO BE REMOVED AS ROCK EXCAVATION AND WILL RECORD THE INFORMATION. THEN DRILL, OR BREAK WITH POWER-OPERATED TOOLS SPECIALLY DESIGNED FOR ROCK EXCAVATION, AND EXCAVATE THE MATERIAL.

E. USE OF EXPLOSIVES SHALL BE PROHIBITED.

F. ROCK EXCAVATION SHALL INCLUDE REMOVAL OF THE ROCK. ALL NECESSARY PRECAUTIONS SHALL BE TAKEN FOR

PROTECTION OF THE WORK, PERSONS, OR PROPERTY. G. ROCK EXCAVATED, IF ANY, SHALL BECOME THE PROPERTY OF THE CONTRACTOR AND MUST BE REMOVED FROM THE SITE AND DISPOSED OF BY THE CONTRACTOR.

H. OVER EXCAVATION RESULTING FROM ROCK EXCAVATION SHALL BE RESTORED AT THE CONTRACTOR'S EXPENSE WITHOUT ADDITIONAL COMPENSATION.

2.0 MEASUREMENT AND PAYMENT FOR ROCK EXCAVATION A. PAYMENT FOR ROCK EXCAVATION WILL BE MADE AT THE RESPECTIVE UNIT PRICES PER CUBIC YARD STATED IN THE PROPOSAL FOR THE ROCK EXCAVATION, AND ALL OTHER INCIDENTAL WORK.

B. PAYMENT FOR ROCK EXCAVATION SHALL CONSTITUTE FULL COMPENSATION FOR ALL WORK SPECIFIED UNDER

C. THE PRICE BID PER CUBIC YARD SHALL INCLUDE ALL EXTRA ROCK EXCAVATION REQUIRED TO PROVIDE SPACE

FOR PIPE BASE OR STRUCTURES WHERE SPECIFIED OR ORDERED BY THE ENGINEER. THE BID PRICE SHALL ALSO INCLUDE ANY INCIDENTAL ROCK EXCAVATION NECESSARY TO WIDEN THE TRENCH FOR INSTALLATION OF BRANCH-LINE FITTINGS.

D. ROCK MEASUREMENT: VOLUME OF ROCK ACTUALLY REMOVED, MEASURED IN ORIGINAL POSITION, BUT NOT TO EXCEED THE FOLLOWING:

1. 12 INCHES (300 MM) OUTSIDE OF CONCRETE FORMS AT FOOTINGS

2. OUTSIDE DIMENSIONS OF CONCRETE WALLS INDICATED TO BE CAST AGAINST ROCK WITHOUT FORMS OR EXTERIOR WATERPROOFING TREATMENTS.

3. 6 INCHES (150 MM) BENEATH BOTTOM OF CONCRETE SLABS ON GRADE.

4. 6 INCHES (150 MM) BENEATH PIPE IN TRENCHES, AND 20 INCHES (457 MM) OUTSIDE PIPE DIAMETER (LIMITED TO 10-INCHES ON EITHER SIDE OF THE PIPE).

E. UNIT PRICES FOR ROCK EXCAVATION INCLUDE REPLACEMENT WITH APPROVED MATERIALS.

F. THIS IS NON-GUARANTEED BID QUANTITY. NO PAYMENT WILL BE MADE FOR LOSS OF ANTICIPATED PROFITS OR OVERHEAD SHOULD THIS ITEM BE DELETED OR REDUCED

G. ROCK OVER-EXCAVATION: NO ADDITIONAL PAYMENTS WILL BE MADE FOR EXCAVATIONS MADE BEYOND THE LIMITS OF MEASUREMENT DEFINED ABOVE. ANY ROCK EXCAVATION BEYOND THE MAXIMUM LIMITS AS STATED ELSEWHERE SHALL BE CONSIDERED ROCK OVER-EXCAVATION. IF ROCK OVER-EXCAVATION OCCURS, FOR ANY REASON, STRUCTURAL BACKFILL OR ENGINEERED FILL, AS DIRECTED BY THE ENGINEER, SHALL BE USED TO REPLACE THE ROCK OVER-EXCAVATED VOLUME AT THE CONTRACTOR'S SOLE EXPENSE. ALL MATERIALS REPLACED BACK INTO ROCK OVER-EXCAVATION AREAS SHALL BE PLACED ACCORDING TO THE SPECIFICATIONS OF THIS SECTION. NO ADDITIONAL PAYMENT SHALL BE ALLOWED FOR SAFETY OR CONSTRUCTABILITY ISSUES CONNECTED TO

H. ANY ROCK MATERIAL REMOVED BEYOND THE PRE-DETERMINED LIMITS SET BY THE ENGINEER SHALL BE CONSIDERED ROCK OVER-EXCAVATION.



ROCK EXCAVATION NOTES

JOB NO. 21B295 DATE 07/25/22

REVISIONS

BELL DESIGN COMPANY, ALL RIGHTS RESERVED. REPRODUCTION OR USE IN ANY FORM
OR BY ANY MEANS -GRAPHIC, ELECTRONIC, MECHANICAL,
ETC. -- WITHOUT MRITTEN
PERMISSION OF BELL DESIGN

COMPANY IS UNLAWFUL AND SUBJECT TO CRIMINAL PROSECUTION.

RECORD DRAWINGS

PORTWAY STORMWATER PIPELINE PROJECT



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6. TRENCH COMPACTION SHALL BE PER HRES STANDARD SPECIFICATIONS. CONTRACTOR

TO DETERMINE THE TYPE OF EQUIPMENT AND METHOD TO ACHIEVE THE REQUIRED

7. COVER SHALL BE AS SHOWN ON PLANS. MINIMUM COVER SHALL BE 3' FROM FINISHED

CONTRACTOR AT NO EXPENSE TO THE CITY AND OWNER.

THE MANHOLES AND BETWEEN MANHOLES AND CLEANOUTS.

GRADE TO TOP OF PIPE.

CITY'S INSPECTOR.

SIZES AND NUMBER OF OPENINGS.

AS REQUIRED UNTIL CUT-OVER.

INSPECTED UNTIL FOUND ACCEPTABLE.

PRIOR TO FINISH SURFACING.

US 160NW, OR EQUAL.

CO.02

COMPACTION. EACH LIFT SHALL BE COMPACTED TO A MINIMUM OF 95 PERCENT OF

THE MAXIMUM DENSITY AS DETERMINED BY THE A.A.S.H.T.O. T-180 TEST METHOD.

9. UNLESS NOTED OTHERWISE, MANHOLE WALLS SHALL BE PRECAST REINFORCED CONCRETE

10. IF DRAINAGE FIELD TILE IS ENCOUNTERED DURING CONSTRUCTION, THE CONTRACTOR

8. SETTLEMENT OF THE FINISHED SURFACE WITHIN THE WARRANTY PERIOD SHALL BE CONSIDERED

TO BE A RESULT OF IMPROPER COMPACTION, AND SHALL BE PROMPTLY REPAIRED BY THE

MANHOLES OF THE SIZE REQUIRED PER MANUFACTURER'S RECOMMENDATIONS FOR THE PIPE

SHALL NOTIFY THE PROJECT ENGINEER AND THE CITY'S INSPECTOR. THE INTENT WILL

11. SEWER LINES SHALL BE LAID IN A STRAIGHT ALIGNMENT AND IN A UNIFORM GRADE BETWEEN

12. THE CONTRACTOR SHALL BE RESPONSIBLE TO DETERMINE, PROVIDE, AND MAINTAIN TEMPORARY

ENCOUNTERED DURING UTILITY CONSTRUCTION. MAINTAIN ALL SERVICE FLOWS. BY-PASS

MAINS AND SERVICE LATERALS. THE LOCATE WIRE SHALL BE ACCESSIBLE AT ALL VALVES. RISERS, MANHOLES, AND SERVICE BOXES. ALL SPLICES AND CONNECTIONS TO LOCATE

BRACING NEEDED TO COMPLETE SEWER INSTALLATION. TRENCH WORK AND CONSTRUCTION SHALL

COMPLY WITH OSHA REQUIREMENTS. EXCESSIVE TRENCH WIDTH DUE TO LACK OF SHORING AND BRACING SHALL BE FILLED WITH STRUCTURAL BACKFILL AT CONTRACTORS SOLE EXPENSE.

BE TO CONNECT ANY FUNCTIONING DRAIN TILE SYSTEM TO THE STORM DRAIN SYSTEM

IN AN APPROPRIATE MANNER. SUCH CONNECTION MUST BE NOTED ON THE AS-BUILT DRAWINGS AND MUST BE APPROVED BY THE PROJECT ENGINEER AS WELL AS THE

13. THE CONTRACTOR SHALL LOCATE & DISCONNECT ALL SERVICE CONNECTIONS THAT ARE

WIRE SHALL BE PROTECTED WITH KING GEL CAPS OR AN APPROVED EQUAL

17. LAMPING AND MANDREL TESTING OF MAINLINE IS REQUIRED PER HRES.

18. VIDEO INSPECTION IS REQUIRED PER HRES AND ODOT STANDARDS.

INSPECTED BEFORE CONSTRUCTION COMMENCES.

GENERAL NOTES

14. 12 GAUGE INSULATED GREEN LOCATE WIRE SHALL BE PLACED DIRECTLY OVER ALL SEWER

15. CITY OBSERVATION AND LOCATE/TRACER WIRE TESTING IS REQUIRED PRIOR TO PAVING.

16. ALL MATERIAL INSPECTIONS AND TESTS ARE TO BE IN ACCORDANCE WITH CITY OF HOOD

ALL SECTIONS FAILING TO PASS THE REQUIRED TESTS AND INSPECTIONS SHALL BE

RIVER AND/OR ODOT STANDARD SPECIFICATIONS, WHICHEVER IS MOST RESTRICTIVE.

LOCATED AND REPAIRED. AFTER REPAIR, THESE SECTIONS SHALL BE RE-TESTED AND

A. ALL EXISTING PIPES SERVING CONNECTIONS FOR NEW SEWERLINES SHALL BE VIDEO

RECONNECTED TO THE NEW REPLACEMENT SEWERLINES SHALL BE VIDEO INSPECTED.

19. ALL TESTS MUST BE WITNESSED AND PASSED BY THE CITY AND/OR ITS AUTHORIZED AGENTS.

21. CONTRACTOR SHALL MAINTAIN ACCESS TO ALL RESIDENTS AT ALL TIMES DURING CONSTRUCTION.

MEDIUM WEIGHT, 6 OZ PER SQYD MIN, GEOTEXTILE FORMED FROM 100% POLYPROPYLENE

STAPLE FIBERS/FILAMENTS, UNO. ACCEPTABLE PRODUCTS ARE ADS 0601T, US FABRICS

B. ALL EXISTING SEWERS BEING REPLACED WHERE EXISTING LATERALS WILL BE

20. WORK HOURS SHALL BE 7:00 AM TO 7:00 PM MONDAY - FRIDAY EXCLUDING HOLIDAYS.

22. ALL GEOTEXTILE FABRIC AND FILTER FABRICS SHALL BE NON-WOVEN NEEDLEPUNCHED

C. ALL NEW SEWERLINES SHALL BE VIDEO INSPECTED AFTER COMPACTION AND

REQUIRED TO ENSURE THAT ALL PAVED AREAS ARE KEPT CLEAN FOR THE DURATION OF THE PROJECT

6. TO MINIMIZE EROSION AND SEDIMENTATION TRANSPORTATION, EARTHWORK SHALL NOT BE PERFORMED WHILE SOILS ARE IN AN UNSTABLE STATE DUE TO PRECIPITATION.

7. THE CONTRACTOR SHALL BE RESPONSIBLE TO HAVE CLEARING LIMITS AND/OR ANY EASEMENTS, SENSITIVE OR CRITICAL AREAS, AND THEIR BUFFERS, TREES, AND DRAINAGE COURSES FLAGGED PRIOR TO CONSTRUCTION. DURING THE CONSTRUCTION PERIOD, NO DISTURBANCE BEYOND THE FLAGGED CLEARING LIMITS SHALL BE PERMITTED. FLAGGING LIMITS ARE TO BE MAINTAINED BY THE CONTRACTOR FOR THE DURATION OF CONSTRUCTION.

8. REMOVE ONLY THOSE TREES AND SHRUBS THAT NEED TO BE REMOVED FOR THE CONSTRUCTION OF ROADS, SIDEWALKS, UTILITIES, AND STORMWATER FACILITIES.

9. ALL EXISTING AND NEWLY INSTALLED CATCH BASINS OR CURB INLETS AFFECTED BY CONSTRUCTION SHALL BE PROTECTED AGAINST SEDIMENT DEPOSITS. AT NO TIME SHALL MORE THAN ONE FOOT OF SEDIMENT BE ALLOWED TO ACCUMULATE WITHIN A TRAPPED CATCH BASIN. ALL CATCH BASINS AND CONVEYANCE LINES SHALL BE CLEANED PRIOR TO PAVING. THE CLEANING OPERATION SHALL NOT FLUSH SEDIMENT LADEN WATER INTO THE DOWNSTREAM SYSTEM.

10. ALL POLLUTANTS THAT OCCUR ON-SITE DURING CONSTRUCTION SHALL BE HANDLED AND DISPOSED OF IN A MANNER THAT DOES NOT CAUSE CONTAMINATION OF STORMWATER SYSTEM.

11.ALL DISTURBED SOIL SURFACES ARE TO BE STABILIZED BY A SUITABLE APPLICATION OF "BEST MANAGEMENT PRACTICES". NO SOILS SHALL REMAIN UNSTABILIZED FOR MORE THAN TWO DAYS DURING THE PERIOD OF OCTOBER 1 THRU JULY 5. FROM JULY 5 THRU OCTOBER 1, NO SOIL SHALL REMAIN UNSTABILIZED FOR MORE THAN 7 DAYS. STABILIZATION OF DISTURBED SOIL AREAS CAN CONSIST OF HYDRO-SEEDING OR HAND-SEEDING, MULCHING, PLACEMENT OF EROSION CONTROL BLANKETS OR PLASTIC. ALL SEEDED AREAS ARE TO BE FERTILIZED, WATERED, AND MAINTAINED TO ENSURE THAT THE REGROWTH OF VEGETATION IS AS SOON AS POSSIBLE. SLOPES STEEPER THAN 3: 1 WILL BE PERMANENTLY STABILIZED USING JUTE EROSION CONTROL FABRIC PROPERLY STAPLED TO THE SOIL & SEEDED WITH MULCH & A TACKIFYING AGENT AS APPROVED BY THE EOR OR CESCL

12. WADDLES OR SILT FENCES SHALL BE PLACED AT THE TOP & BOTTOM OF CUT & FILL SLOPES & AT MID SLOPES SO THAT THE DISTANCE BETWEEN WADDLE ROWS IS NO MORE THAN 6 FEET.

13. ALL TEMPORARY SEDIMENT AND EROSION CONTROL "BMP'S" SHALL BE REMOVED WITHIN 30 DAYS AFTER FINAL SITE STABILIZATION IS ACHIEVED OR AFTER THE TEMPORARY "BMP'S" ARE NO LONGER

14.THE CONTRACTOR SHALL BE RESPONSIBLE FOR POLICING THE JOB SITE DAILY AND MAINTAINING THE EROSION/SEDIMENT CONTROL MEASURES THROUGHOUT ALL PHASES OF CONSTRUCTION. AN INSPECTION LOG SHALL BE KEPT AND MADE AVAILABLE TO THE CITY OF HOOD RIVER. THE POLICING AND MAINTENANCE SHALL INCLUDE, BUT NOT BE LIMITED TO:

* VERIFYING THAT ALL AREAS ARE GRADED SUCH THAT ALL RUNOFF IS DIRECTED TO A SEDIMENTATION DEVICE BEFORE DISCHARGE TO SURFACE

* REMOVAL OF TRAPPED SILT AT SILT BARRIERS, SILT TRAPS, OR POINTS OF ACCUMULATION.

* ADDITIONAL PROTECTIVE MEASURES DUE TO JOB SITE OR WEATHER CONDITIONS AS

* MONITORING OF VEHICLES LEAVING THE SITE TO MINIMIZE TRANSMISSION OF LOOSE SOILS TO THE PUBLIC ROADWAYS.

* PLACING OF THE STRAWBALE SEDIMENTATION BARRIER AT THE ENTRANCE DURING

REQUIRED BY THE CITY OF HOOD RIVER ENGINEERING STANDARDS

PRECIPITATION OR AT THE END OF THE WORKDAY BEFORE LEAVING THE SITE. * VERIFY THAT ALL PROPERTIES ADJACENT TO THE PROJECT SITE ARE PROTECTED FROM SEDIMENTATION DEPOSITION. THIS MAY BE ACCOMPLISHED BY INSTALLING PERIMETER

CONTROLS SUCH AS SEDIMENTATION BARRIERS, FILTERS OR DIKES, SEDIMENTATION BASINS/TRAPS, OR BY A COMBINATION OF SUCH MEASURES.

15.CUT AND FILL SLOPES SHALL BE DESIGNED AND CONSTRUCTED IN A MANNER THAT WILL MINIMIZE EROSION. SLOPES SHALL BE STABILIZED IN ACCORDANCE WITH EROSION/SEDIMENT CONTROL NOTE 11. SLOPES FOUND TO BE ERODING EXCESSIVELY WITHIN TWO YEARS OF CONSTRUCTION MUST BE PROVIDED WITH ADDITIONAL SLOPE STABILIZING MEASURES AT THE CONTRACTORS EXPENSE. THESE MEASURES MAY CONSIST OF ROUGHENED SOIL SURFACES, INTERCEPTORS OR CUT-OFF TRENCHING, DIVERSIONS OR TERRACES, ROCK ARMORMENT, EROSION CONTROL BLANKETS, TEMPORARY OR PERMANENT CHANNELS, ADDITIONAL VEGETATION, OR PIPE SLOPE DRAINS AS REQUIRED BY THE CITY OF HOOD RIVER ENGINEERING STANDARDS UNTIL THE PROBLEM IS CORRECTED.

16. THE ESC MEASURES ON INACTIVE SITES SHALL BE INSPECTED AND MAINTAINED A MINIMUM OF ONCE A MONTH OR WITHIN 24 HOURS FOLLOWING A STORM EVENT.

17. DURING TRENCHING ACTIVITIES, THE CONTRACTOR SHALL ADHERE TO THE FOLLOWING PRACTICES:

* WHERE FEASIBLE, NO MORE THAN 100 FEET OF TRENCH SHALL BE OPEN AT ONE TIME.

* WHERE CONSISTENT WITH SAFETY AND SPACE CONSIDERATIONS, EXCAVATED MATERIAL

SHALL BE PLACED ON THE UPHILL SIDE OF TRENCHES. * TRENCH DE-WATER DEVICES SHALL DISCHARGE INTO A SEDIMENT TRAP OR SEDIMENT POND.

GENERAL EROSION CONTROL NOTES

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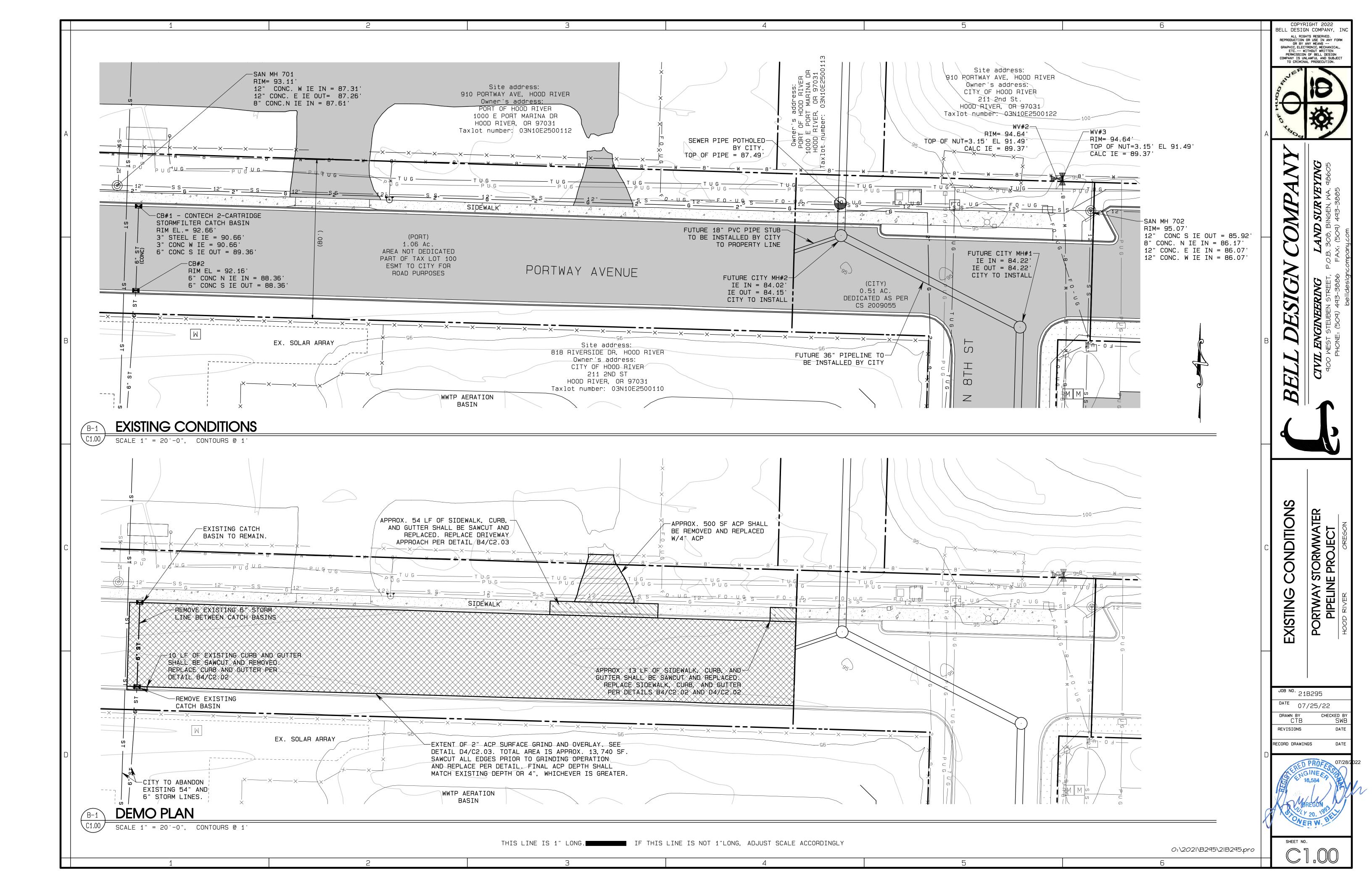
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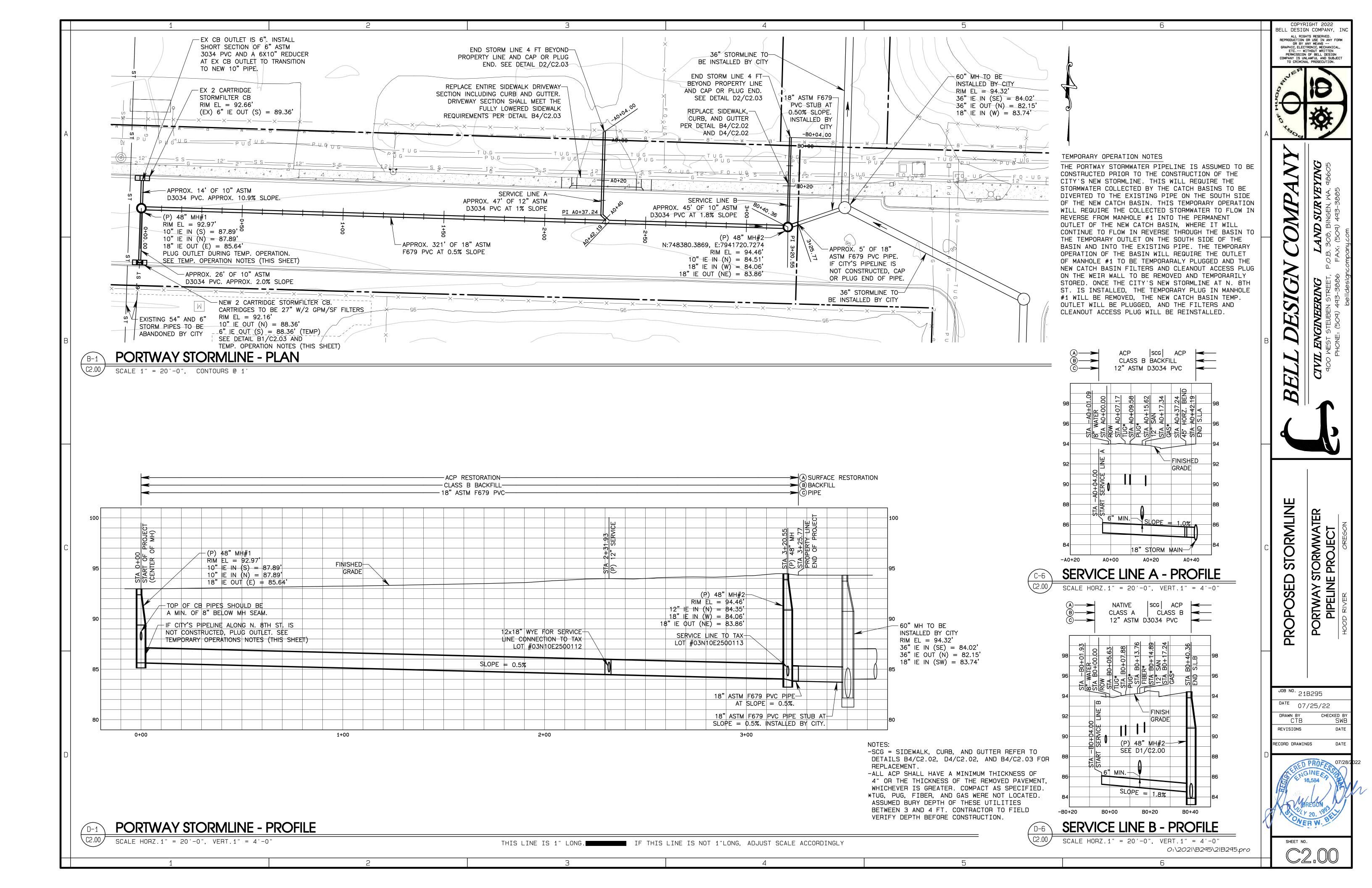
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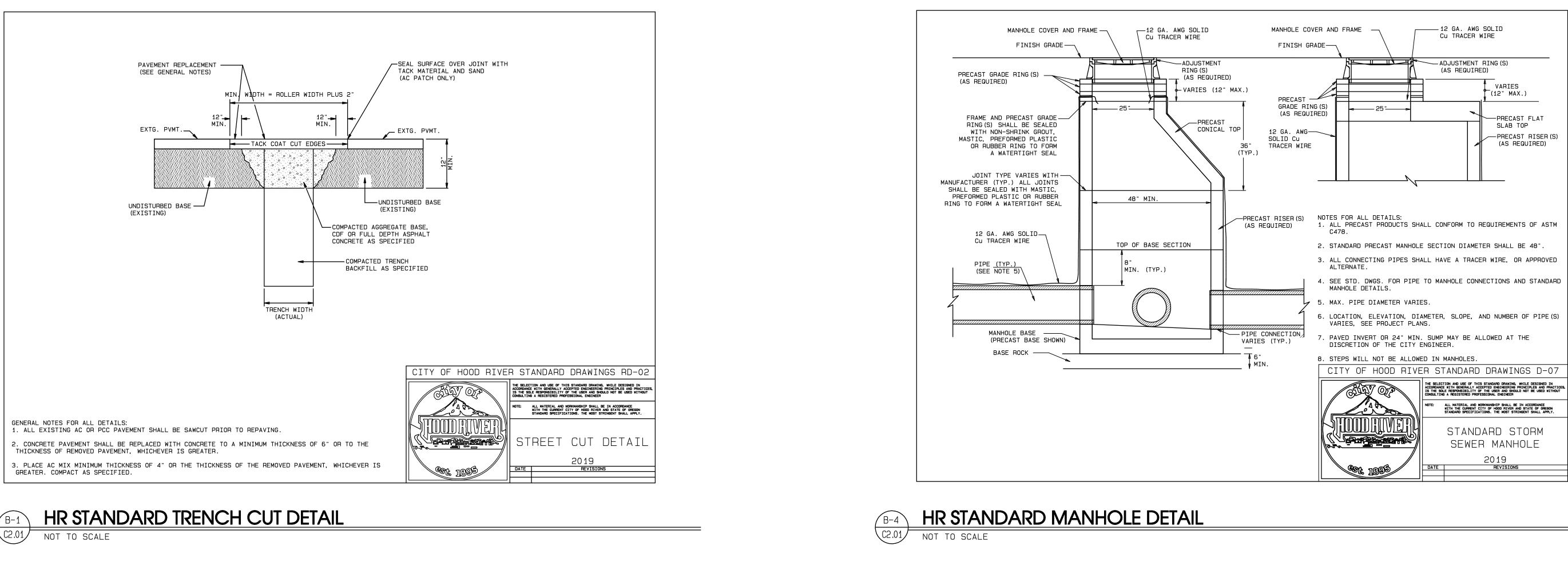
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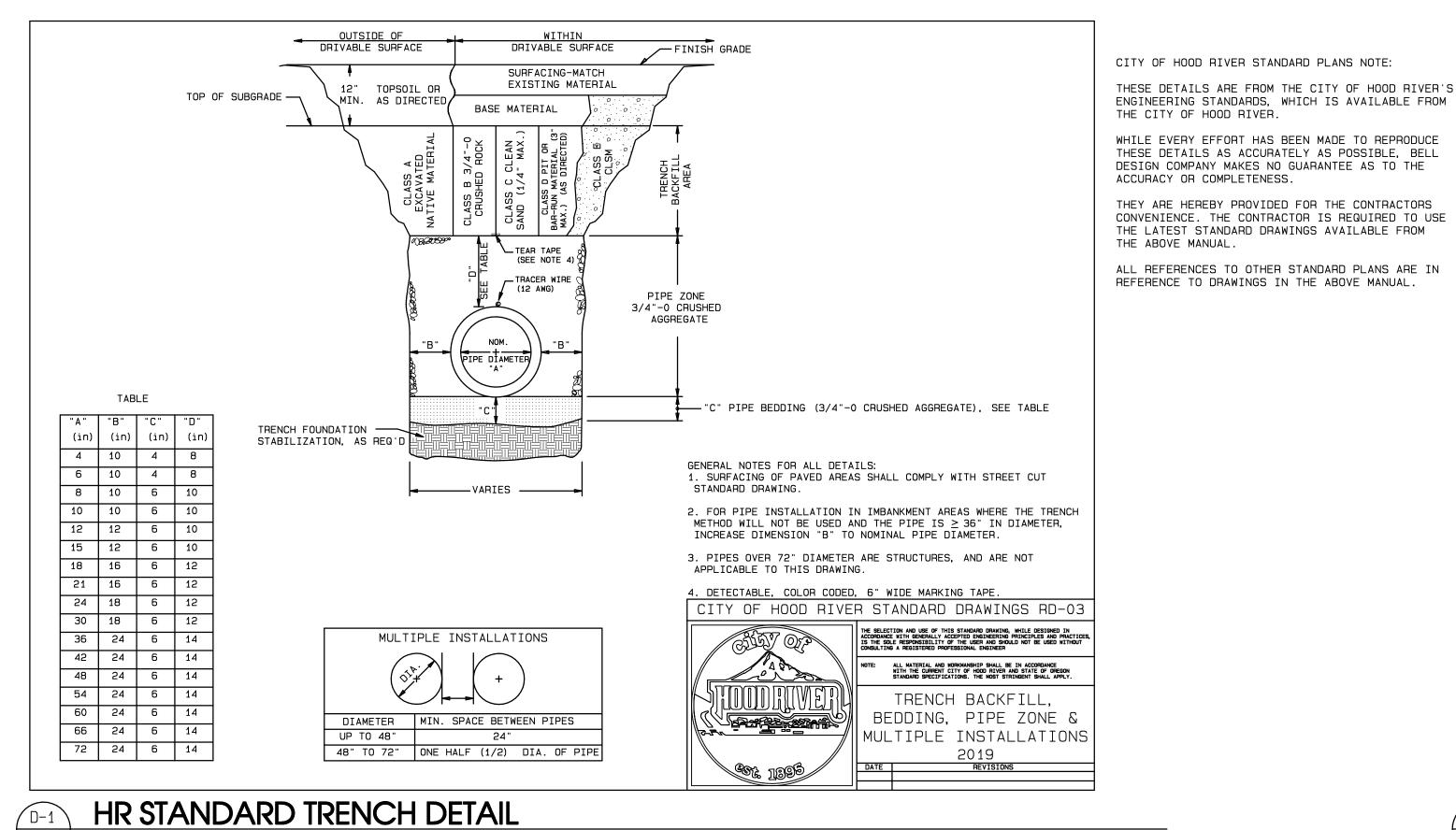
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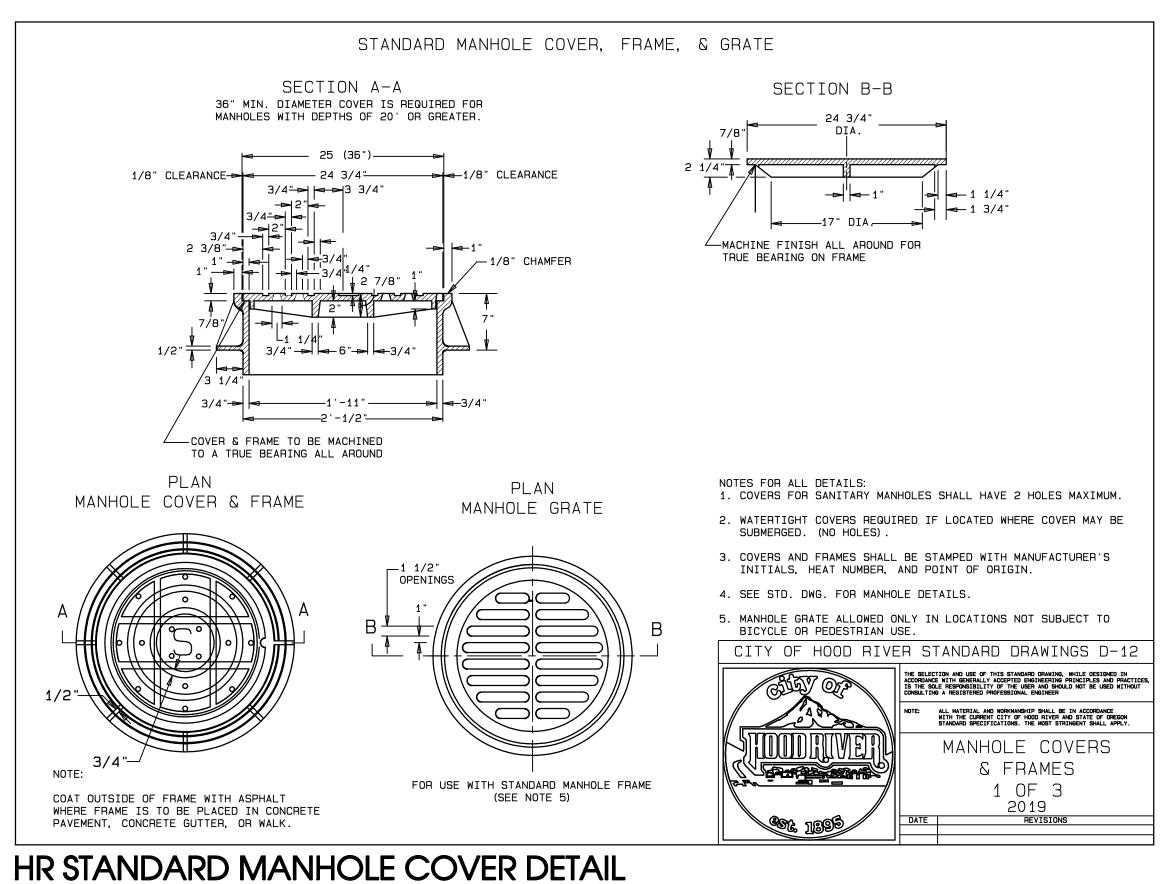




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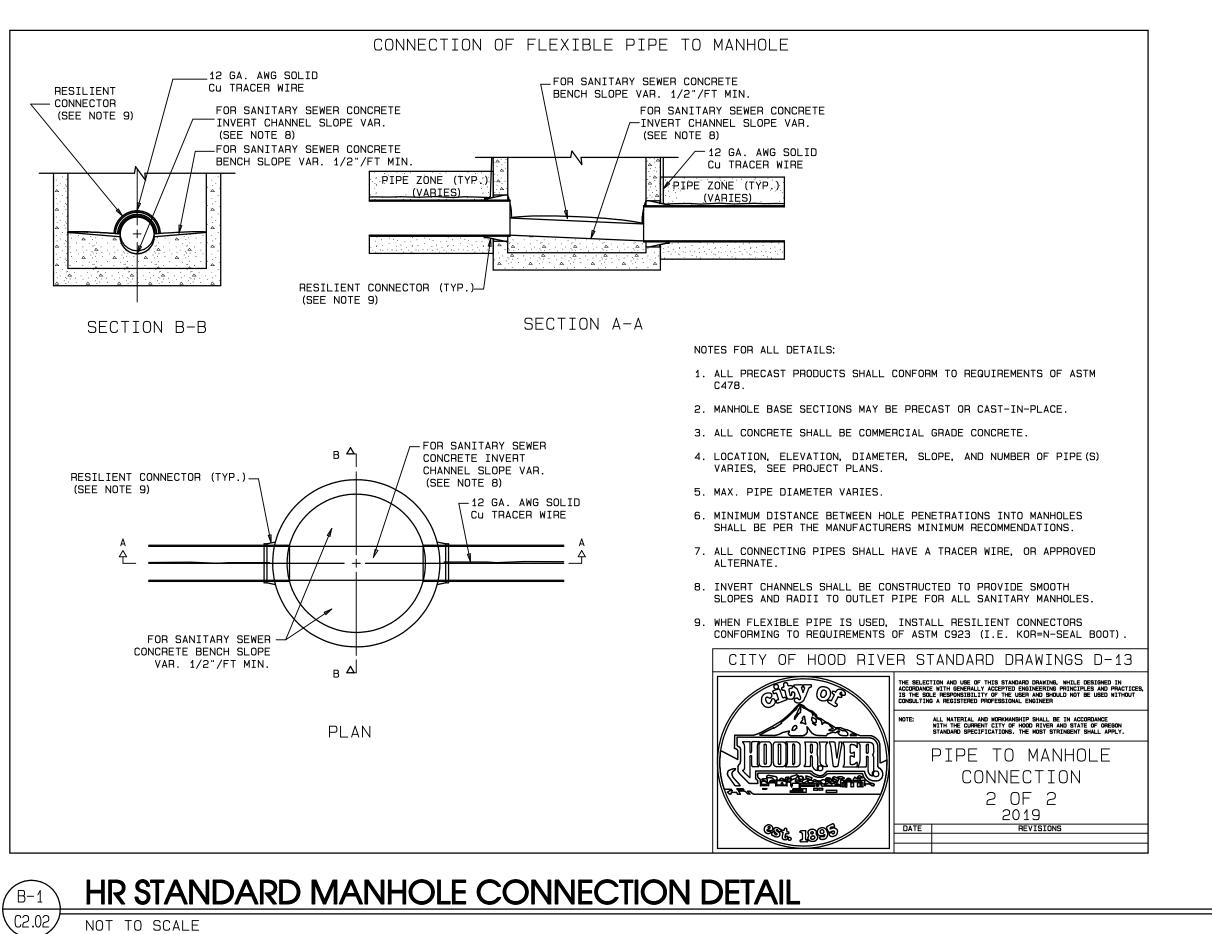
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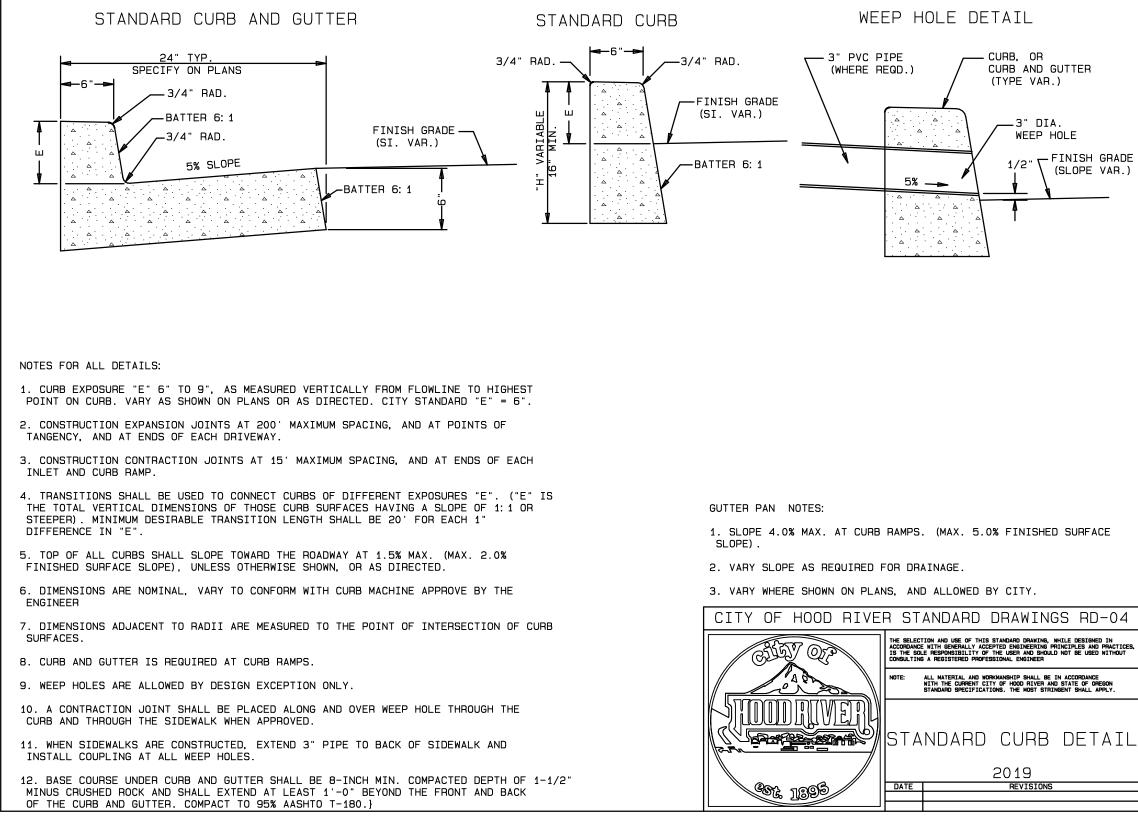
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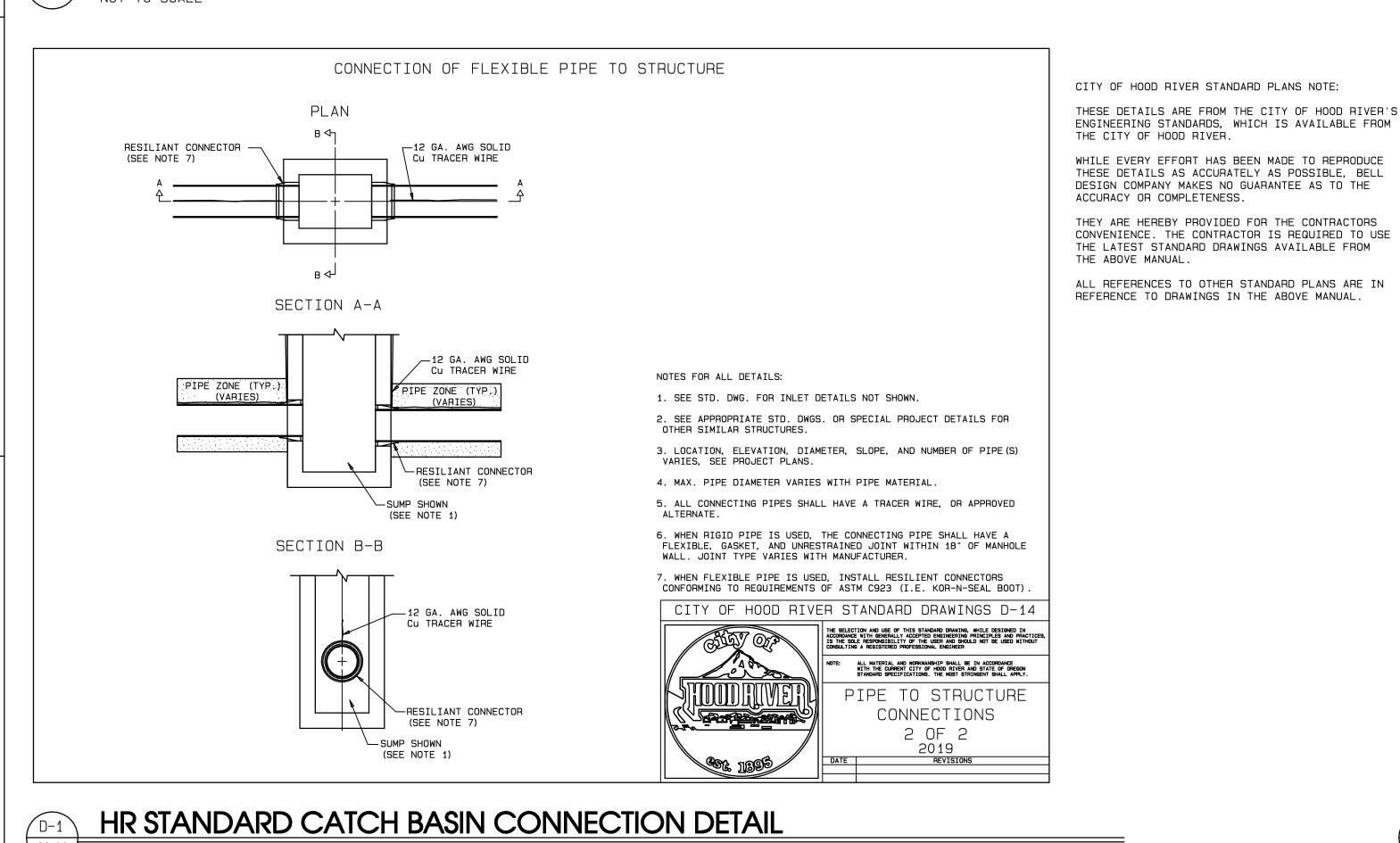
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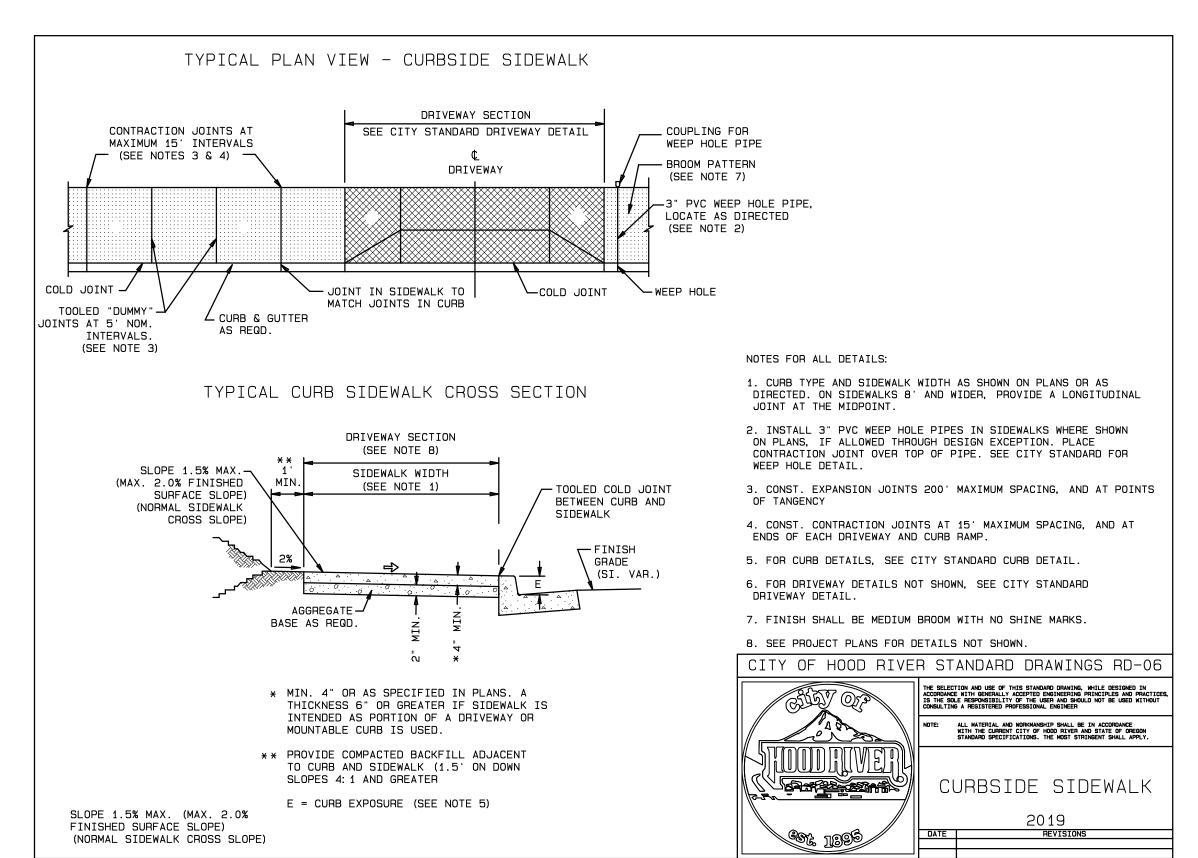
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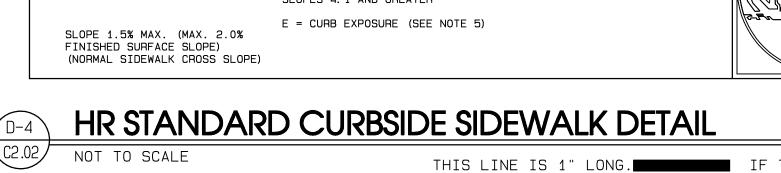






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